

TELEPHONE:
AVENUE 3466 & 7.

A. LUSI.
SHIP & INSURANCE
BROKER

CODES:
SCOTT'S (10TH. ED.)
WATKINS, A.B.C. (5TH. ED.)
MARCONI INTERNATIONAL.

BOE.

REF. F
REF. AL/AVJ



TELEGRAMS:
FOREIGN: ANGELLUSI, LONDON.
INLAND: ANGELLUSI, STOCK, LONDON.

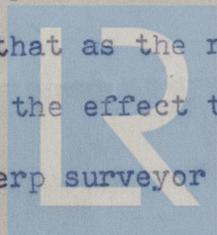
23, ST. MARY AXE,
LONDON, E.C.3.

29th January, 1935.

The Secretary,
Lloyd's Register of Shipping,
71, Fenchurch Street,
E. C. 3.

Dear Sir,

I beg reference to your letters of the 24th and 28th inst. relative to the steamer "ATTIKOS". In reply to the former I beg to point out that, as I also mentioned in my letter of the 22nd inst, I was distinctly under the impression that all the requirements had been complied with. Indeed on looking up my notes I find that on the 6th August 1934 my surveyor Mr. Polychroniadis who had attended to the ship at Antwerp wrote that he went further into the matter of baring surfaces with the surveyor at Antwerp and we were to communicate with the Society and this was done on the following day. I find that on the 8th August I wrote Mr. Polychroniadis at Antwerp that his assistant had seen you and that you were prepared to leave things undisturbed if the surface was $2\frac{1}{2}$ inches but that as the report you had from the Rotterdam surveyor was not to the effect that the surfaces were $2\frac{1}{2}$ inches, you wrote your Antwerp surveyor to measure the



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faces again and that if they proved to be $2\frac{1}{2}$ inches the matter would be in order but otherwise we should have to do further work.

There is no further record relating to this matter in my office and for that reason I was all along under the impression that the surveyor, acting on your instructions referred to above, measured the surfaces and reported to you and that presumably you were satisfied with the position.

It was for this reason that on the 22nd inst. (when the agents pointed out that the new certificate was not in the Captain's possession) I wrote that the requirements had been complied with and requested you to issue the certificate.

As you will observe, there has been a very unfortunate misunderstanding which nearly caused me considerable trouble-unnecessarily, there has never been any question of avoiding to comply with the requirements so far as I am concerned.

Following our conversation of the 24th inst. I instructed Messrs. Fergusson & Law to attend to the matter and I now gather from them that the work required by the Society involves much more than would appear to be necessary from previous records and your letter of the 24th inst.

In particular reference to the last paragraph of your letter of the 24th inst. I beg to confirm that at the time it was quite possible to do the work required as the steamer had already been fully loaded in two of the holds.

In reply to your letter of the 28th inst. I beg to say that the ship is about to complete loading at Cardiff for Gibraltar and the

W 478 - 0127(213)

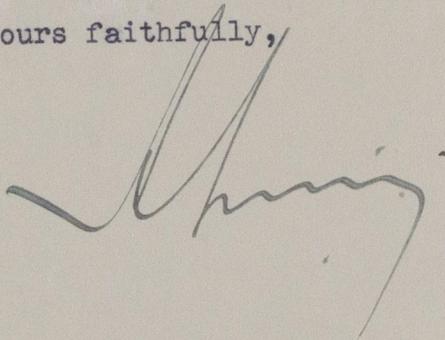
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DATE 29th January, 1935

ention is to bring her back to this side either in ballast or with
argo of ore ~~for~~^{from} a near port and I shall advise you in good time
to when and where the necessary work will be done.

Yours faithfully,

A handwritten signature in dark ink, appearing to be 'L. King', written in a cursive style.

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Referred to the Chief Ship Surveyor
(Freeboard).

MB

also for *20 JAN 1935*
also for det. repairs to gear.

Noted. MB 30.1.35



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