

# Report of Survey for Repairs, &c., of Engines and Boilers.

Date of writing Report 4/11/38 When handed in at Local Office 4/11/38 (Received at London Office -5 NOV 1938)

No. in Reg. Book 11672 Survey held at London Port of London

on the Machinery of the Wood, Iron or Steel SC. CANTABRIA (EX ALFONSO PEREZ) Date, First Survey 28 October Last Survey 31 October 1938 (No. of Visits THREE)

Gross Tonnage 5649 Net Tonnage 3344 Vessel built at Vancouver By whom J. Laughlan Engines made at Greenock By whom J. G. Mcneil When 1919-20

Nominal Horse Power 532 Boilers, when made (Main) 1919 Owners A. Y. Perez Managers  Owners' Address  (if not already recorded in Appendix to Register Book.)

No. of Main Boilers 3 No. of Donkey Boilers 1 Team Pressure in Main Boilers 180 in Donkey Boilers 1 If Surveyed Afloat or in Dry Dock Yes (State name of Dock.) Millwall S.D. Particulars of Classification (which must be inserted precisely as in Register Book & Supplements.)

Port Santander Voyage  Particulars of Classification (which must be inserted precisely as in Register Book & Supplements.)

Particulars of Examination and Repairs (if any) Damage

Periodical Surveys, when held, must be reported in detail and seriatim in the terms of the Rules. State clearly the cause of Repairs, if any, and, in detail, the nature and extent of Examinations and subsequent Repairs. Repairs on account of Damage (the cause of which must be stated) should be separated from Repairs due to other causes; and besides being detailed in the body of the report, should be briefly summarised at the end of the report. State also the dates and initials of any letters respecting this case.

Where the Surveyor has not made a special damage report he is required to state whether he offered his services for this purpose, and why they were declined not required

Was a damage report made by anyone else? If so, by whom? Underwritten Surveyors

Did the Surveyor personally go inside each Main Boiler separately and make a thorough examination at this time? Yes

Did the Surveyor personally go inside each Donkey Boiler separately and make a thorough examination at this time? Yes

Was this not done, state for what reasons?

What parts of the Boilers could not be thus thoroughly examined?

What special means, in the absence of internal examination, were adopted by the Surveyor to assure himself of the thorough efficiency of those parts of each Boiler?

State latest date of internal examination of each boiler. Present condition of funnel(s) Yes

Did the Surveyor examine the Safety Valves of the Main Boiler? To what pressure were they afterwards adjusted under steam? Yes

Did the Surveyor examine the Safety Valves of Donkey Boiler? To what pressure were they afterwards adjusted under steam? Yes

Did the Surveyor examine all the manholes, doors and their fastenings of the Main Boilers? Yes and of the Donkey Boilers? Yes

Did the Surveyor examine the drain plugs of the Main Boilers? Yes and of the Donkey Boilers? Yes

Did the Surveyor examine all the mountings of the Main Boilers? Yes and of the Donkey Boilers? Yes

Has screw shaft now been drawn and examined? Yes Is it fitted with continuous liner? Yes Is an approved appliance fitted at the after end of the shaft to permit of it being efficiently lubricated? Yes

Has shaft now been changed? Yes If so, state reasons fract. in way of keyway

Has the shaft now fitted been previously used? Yes Has it a continuous liner? Yes Is an approved appliance fitted at the after end of the shaft to permit of it being efficiently lubricated? Yes

State date of examination of Screw Shaft 28/10/38. State the distance between lignum vitae or bearing metal of stern bush and top of after bearing of screw shaft Correct

Engine parts, when referred to by numbers, should be counted from forward. Is electric light and/or power fitted? Yes

So, did the Surveyor examine the generators, motors, switchgear, cables and fuses? Yes

Has the insulation resistance of the generators, circuits and apparatus been tested and found to be not less than 100,000 ohms? Yes

If the Survey is not complete, state what arrangements have been made for its completion and what remains to be done. Damage stated to have been caused by vessel grounding at Staines in R. Thames on 3/10/38.

How done: Examine propeller stern bush screw shaft and all sea cocks valves & fastenings.

Spare shaft marked HEAT 1850 C15 LLOYD 265 25/10/18/N. stored in lathe machined on taper & fitted to propeller. i.e. this shaft was previously fitted with new (C.L.). Bush unroaded.

Sea cocks valves unchailed as necessary.

General Observations, Opinion, and Recommendation: The machinery of this vessel as far as now seen is eligible in my opinion to have fresh record of H.S.(C.L) 10,38.

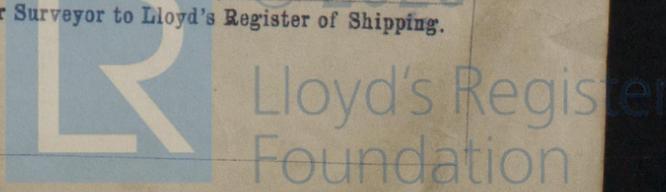
Fees applied for -5 NOV 1938 Received by J.L. Smith Engineer Surveyor to Lloyd's Register of Shipping.

Committee's Minute FRI 11 NOV 1938

signed J.L. Smith

Insert Character of Ship and Machinery precisely as in the Register Book

Is a Certificate required? If so, to be sent to



W471-0007

foundry  
Screw shaft changed

This is a digitized copy  
of a document from the  
Lloyd's Register Foundation

\$10.38

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4/2/38

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